



Risborough and District Model  
Railway Club

**Jul-Sept 2015 Summer**

# **FOOTPLATE**



## Who's who!

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## WELCOME

Welcome to the summer edition of Footplate and welcome to those who have just joined the club. Railex is over and we can relax and enjoy our modelling. Railex is the biggest event in our club calendar and once again David and his team delivered an excellent show. I was particularly impressed with the 2mm and N exhibits but the large layouts are always impressive. Every year Railex is a great show, one I would happily pay to see. I also need to thank all those who helped in setting up, stewarding and clearing away. The marking out was very efficient and we had exhibitors setting up earlier than planned. Thanks to the car park stewards who have a difficult time with a lot of cars and not enough spaces. Every year we make some improvements to our club stand. This year we had more panels to construct it from and concentrated the display on our 3 club projects.

These projects are all progressing but still have plenty to do: Aylesbury Town is being redesigned again to be much larger incorporating the whole station so

if you are interested in helping talk to David or Phil. Aylesbury High Street (or should that be Aylesbury LNWR?) needs rewiring of the final board and some refurbishment to buildings, talk to Tim if interested in helping. Okehampton is getting to the scenic stage so we are making trees and will also look at hedges and grass, if interested talk to James.

Paul

## From the Internet

The Bodmin and Wenford railway

<http://www.youtube.com/watch?v=URLRFUU8Eyg>

70000 "Britannia" on the The Cumbrian Fells Express, 30th May

<http://www.youtube.com/watch?v=Ew1opA6Y0n4>

Making a landscape with Busch scenic products

<http://www.youtube.com/watch?v=LVkTQqFVITw>

45690 "Leander" on the Fellsman at Ais Gill 19th May

<http://www.youtube.com/watch?v=Hu3KsOEl6GA>

A nice steam show from Germany and the Netherlands

<http://www.youtube.com/watch?v=0KOKcchYO9o>

## Rubbish and Recycling

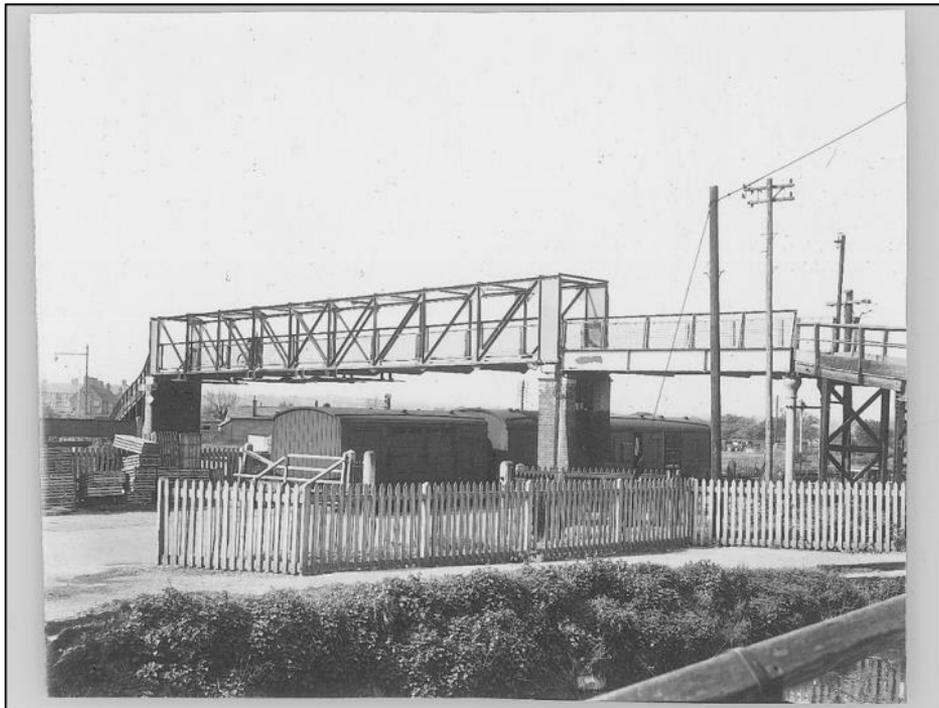
Please remember to put a black plastic bag in the dustbin before use and empty it when full. There is a wheeled metal bin at the end of the community centre when out bags are full.

Recycling will be collected each club night, this includes card, plastic bottles and cans.

Front cover: 46115 Scots Guardsman at Carlisle, John Casson

# AYLESBURY – A BRIDGE TOO FAR

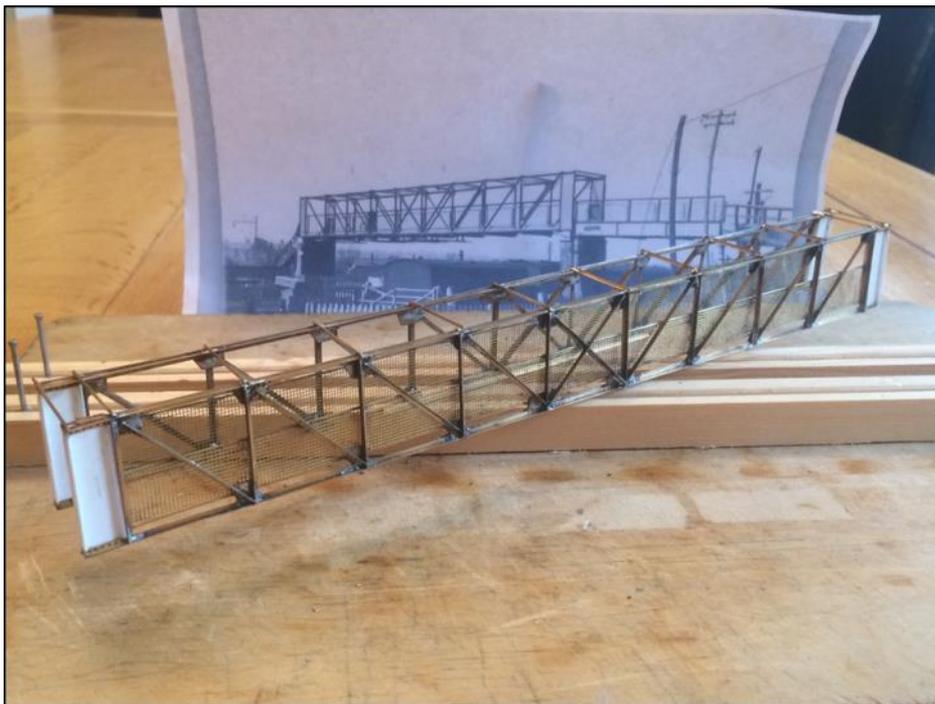
“I need something to get my teeth into”, I said. In hindsight, I would have been better off with a Werther’s Original. Nevertheless, I decided to model the footbridge at the north end of Aylesbury station.



The actual bridge itself has undergone a number of reincarnations. David Lane managed to obtain the original drawings. Unfortunately, the original contractor decided to do his own thing, the span being some 10ft longer than the drawing depicted. Fortunately, the piers remained relatively true to form. Armed with the drawing and a number of photographs from Flickr, I decided to model the span as it stood in the 1950s. The span consisted of a square ended Pratt Truss made up from equal angles, with universal beams at each end. There were nine square sections to the truss and I estimated that each section was 8ft high, giving a total span of 72ft. Much to the delight of Eileen’s Emporium, I used 1mm brass equal angles for the main ties and struts with 0.5mm for the secondary members. By measuring the brickwork below the universal beams, I calculated the beam size as a 686 x 254; which were constructed in suitably sized Plastistruct sections.

I drafted the span to scale using Microsoft Visio software. I laid out a print of the span on an old breadboard and proceeded to set out the main brass cross beams securing them with steel pins. I had not undertaken any soldering for nearly thirty years. For an

intricate piece of work of this nature, I decided to bite the bullet and invest in a decent iron. I plumbed for an Antex 690SD which offered variable temperatures shown on a digital display. I proceeded to solder the uprights to the main beams followed by diagonals. My attempts at soldering were embryonic, being that there appeared to be more solder than brass. I frustratingly chased joints around the truss like a demented Pac Man. The second span benefited from both practise and a slightly higher temperature of 300°C. The gusset plates were replicated using 0.5mm thick brass plate. Paper templates from the design drawing were stuck to the plate and the sections cut out individually with a metal cutter. It is interesting to note that none of gusset plates are the same; I suppose this is a reflection of the post war year's construction quality. To prevent pedestrians hanging (or jumping) through the truss, the bridge was fitted with a mesh screen up to shoulder level. I recreated this using 1mm square Scale Link fret soldered to 0.3mm equal angle.



I expected the first outing of my efforts in the Clubhouse to be greeted with applause and choruses of “for he’s a jolly good fellow”. This was not to be, with the only comment being, “where are the rivets?” With renewed determination, the rivets were recreated using Archer water-slide transfers. Despite the small scale, these proved very effective. However, in future, I will apply the rivets just before the final pint coat, as some of the detail was lost.

I constructed the deck using 1mm thick Plasticard and brass composite; the brass section enabling the deck to be soldered to the bridge trusses. The asphalt surface was recreated using wet and dry paper. The truss (minus the deck) was sprayed with Tamiya primer followed by a finish coat of Humbrol Light Grey. I carefully soldered

the deck to the trusses and fitted the smoke deflectors which were constructed from brass plate and angles. Many thanks to Roy Norton, for the tip of drilling a hole through the deflector plate to enable the solder to run through to the supporting angles. Brass tacks were also soldered to the composite deck to enable positioning on the brick piers. The whole structure was suitably weathered with a light spray coating of Humbrol matt black. The finishing touches to the structure being a couple of train-potters.

My attention now turned to the brick piers. Once again, using Microsoft Visio software, I drafted the design based upon the original drawing and historic photographs. As the intention was to utilise the laser cutter, the design had to be drafted in its constituent parts. The drawing was converted into DXF format and sent to David Lane for final tweaking. A week later, I was presented with the output from the laser cutter in the form individual pieces like an Airfix kit. The quality of the finished product is truly excellent, right down to the intricate masonry work which had a curve on the east pier. Details of this nature would be extremely difficult to recreate in Plasticard. I bevelled the edges of the constituent wall pieces using a rotary sander. This ensured flush edge to the wall corners. Furthermore, David Lane had ingeniously included a mounting hole through each of the pieces to ensure that they lined up during the final assembly. The piers were painted with a Humbrol Orange Lining and Leather mix to match the actual finish. The finished article was weathered with matt black and stains of concrete to represent calcium deposits.



In summary, this has been a very satisfying first contribution to the club layout. My

introduction to the club and this initial and challenging project has renewed my interest in railway modelling. In hindsight, there are things I would have done different. Although the end product is not a perfect recreation of the original, it looks right and more importantly puts a smile on my face. Now, where are those Werther's Originals?

Gary

# **Ticket to Ryde**

Based in Shanklin (Holiday July 2014) and with the hotel only 15mins walk from the station, I decided that whilst on holiday we would travel the Island line operated by Southwest Trains. The photo bellows shows the train at Shanklin station.



Shanklin is the southern terminus of the line, as the route to Ventnor was closed back in the late 1960s. Though there was talk in the mid 1990s of reopening the line to Ventnor, but this is now not cost effective. The trains that operate the Island Line are made up from 2 car ex London

Underground 1938 tube stock (class 483). Back when I was last on the island in the late 1980s the stock was formed of ex London Underground tube stock built between 1923 and 1934 (class 485 and class 486).

We pause at Lake, a relative new station on the island that was opened in 1987, this is just a simple platform made of wood.

We then arrive in the up platform at Sandown, the island platform has now lost it's tall signal box, this happened when the signalling control for the whole line was taken over by Ryde St. John's Road. The small engineer's yard here has seen no use for a number of years, as now the maintenance of the line is all done by road vehicles, a class 03 was used on the island for departmental duties, but these ceased back in 1993. This station used to be a junction for Newport, but this closed back in the late 1950s. Phillip Hutchings has produced a great model of Sandown in 3mm scale, set on a summer Saturday in the early 1950s, which you may have seen at either Risex 2014 or Railex 2014.

After we leave Sandown we were then up to the maximum line speed of 45 miles and hour, and you don't half get thrown around a bit, this seems worse than the tube in London!

Brading our next station stop, here the island platform though not in use, has been kept in good order as well as the main station building which holds a



small visitors centre, there is access to the island platform and the closed signal box, which you can get a tour of on certain days. This was another junction station on the island with a branch to Bembridge again closed in the late 1950s. There has been talk here of putting in a passing loop to allow the trains to run every half hourly rather than at 20min and 40mins pass the hour, thus allowing a more even schedule.

As it was early in the day we did not stop at Smallbrook Junction, as there was no Isle of Wight Steam Railway trains to connect with, this is the newest station on the line and can only be reached by train. At Ryde St. John's Road we pass a train going south, here on the right hand side of the line the workshops remain to perform services and maintenance to the tube stock, on the left hand side used to be the running sheds for the steam trains, though this has long been demolished and now is the station carpark. Semaphore signals still control the station from the only functioning box on the line.



We leave our train at the next station Ryde Esplanade, to go to a photo vantage point which is a footbridge over the tracks to the hovercraft port. The photo on page 8 shows the train crossing over to the down line just after it has left Ryde Esplanade station (behind me), the line is double track from here to just before Smallbrook Junction station, though there is unused double track right up to Ryde Pier Head station. The tunnel that the train is about to enter is below sea level and usually floods during the winter.

We then went back to Ryde Esplanade station, and took a trip up to Ryde Pier Head station, we did not get off as the train only pauses there for a couple of minutes to allow passengers to catch the Ryde to Portsmouth Catamaran service. In the stations heyday during a summer Saturday in the 40s/50s there would have been 4 platform faces serving the trains on the island, with the system of the engine from a preceding train taking out the stock from one that had just arrived. The photo on page 9 is taken from Ryde Esplanade station looking towards the Pier Head station in the background, back in the late 1980s the line on the right would have been as a shuttle service during peak times, but as can be seen this has not seen use for a number of years.

Then is we set of back down the line to the start of the journey at Shanklin, where it was time for lunch and a couple of beers at The Crab pub.

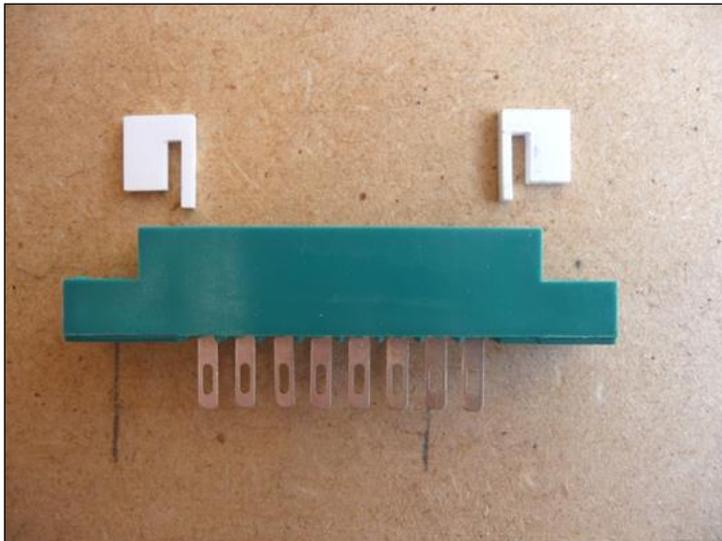
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## Fixing the Tortoise shorting problem

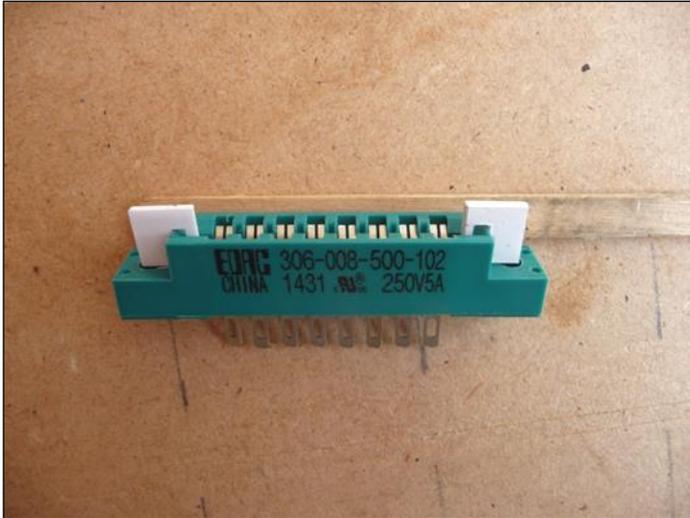
The Tortoise motors suffer from an annoying problem of potential shorts, which on DCC tends to stop running. We have found this many times on Okehampton.

The Tortoise uses a connector which is not a tight fit on the PCB sticking out of the Tortoise housing. As a result, if the connector slides sideways a little, the connector's pins can touch two PCB copper strips at the same time.

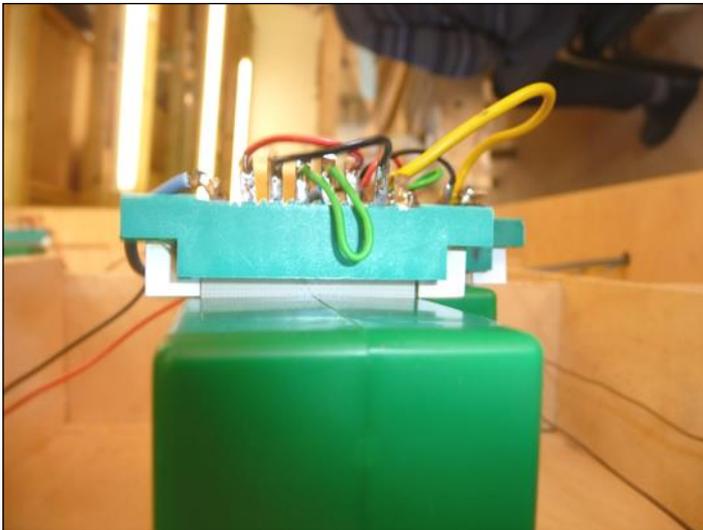
The connector has about 2 mm of sideways movement due to the fact that the slot in the connector is quite a lot longer than the PCB. I designed a small clip which would locate at the ends of the connector's slot, thus preventing the PCB from sliding along the connector. The clip is cut using our laser cutter out of 1.5mm rowmark.



It is sized so that it does not interfere with the Tortoise housing when the connector is pushed home, but its design stops the clip falling out. A little bit of glue on the outside of the clip holds it in place if the connector is removed.



When the connector is installed you get this:



If anyone wants some clips, please let me know. I'll try to keep a number of them at the club with the spare Tortoises.

James

# Railex 2015



Above: Aberbeeg, 7mm scale.  
Below: Addison Park. 3mm scale.

Right upper: Frecclesham, 7mm scale.  
Right lower: Ballyconnell Road, 3mm scale.









Left, top to bottom: 82G (7mm), Corris (1:48th) & Warren Lane, 4mm OO

Above: Grindley Brook, 7mm scale.

Below: Harton Gill, P4.





Above: Whatlington, 3mm scale.

Below: Portsea, 3mm scale.





Above: Loch Tat, N gauge.

Below: Melton Mowbray (North), N scale.





Warren Lane team and the best model in show, judged by Chris Nevard below.





Above: the Black Country Blues team, the winners of the best layout, judged by the exhibitors.

Below: Black Country Blues.

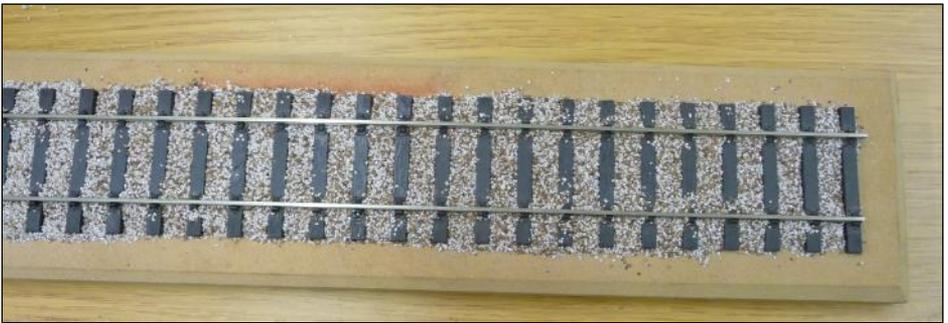


# Okehampton Report

Finally we have completed all the replacement track ends, and have re-aligned all the track on the 6 boards in the shed. All wiring is complete, and working with one minor exception. One point still needs its green wire soldered to the frog!

Once that was done, all track was sprayed with a rust colour on the rails, and then a coating of track dirt to make the sleepers and chairs look the right colour. What a difference that made! Thanks to Tim for bringing his spare compressor on a few days. His help and experience has been invaluable.

We have been experimenting with ballast. We have chosen the medium ballast from Woodland Scenics as it is light and easy to use. The light grey is the bulk of the material with some brown and buff mixed in. It's a recipe arrived at by Pete who spent a happy evening at the club playing with different amounts until we all agreed it looked great. We tried it out on a test track of mine.



The back scene is also being worked on, with us carving the blue foam into a better shape across all the boards. There is still some work to do, but at last the boards are looking good.

Russ has been creating the footbridge which is looking great. Examples were on show at Railex. John is back with us after a few weeks away looking after family, and working on the engine shed, and Jennifer has designed and laser cut all the roof parts for the station building. Must admit I can't wait to see it.

Shed days continue when I'm in! External events contrive to use up my time, but hey. Sometimes family and enjoying myself comes first!



Cheers, James.

## Articles for publication in Footplate

Articles can be on paper or in electronic form (preferred) with minimal formatting, preferably doc, txt or rtf. Digital photos should be at as high a resolution as possible to allow editing and good print quality.

We would like to hear about your railway and modelling interests, places you have visited or models you have bought or made. Reviews of models, gadgets, books etc. are particularly welcome.

Articles should be sent at least 1 month before publication dates, i.e. beginning of March, June, September and December for publication in January, April, July and October.

## **Modelling Saturdays**

The following dates have been booked 09.00 to 17.00

Jul 18, Aug 8,  
Sep 12, Oct 17 (Openday), Nov 14, Dec 12.

I try to arrange modelling Saturdays not to clash with other events but this is not always possible. If you are involved in or know of events that are likely to be of interest to our members then let me or Ant know so we can put them in the diary.

## **Note new date for July**

Paul

## **Test Track Nights**

Here is the list of proposed test track nights. If you want to make use of the test track then you need to get it out and set it up. Don't wait for someone else to do it. We will also have the small meeting room booked to provide more space.

Jul 17, Aug 14, Sep 18, Oct 16, Nov 13, Dec 11.

## **Laser cutting materials**

In stock we have:

0.5, 0.75, 1, 1.5 & 2mm white plastic (Romark) with some 1.5 & 2mm in black. Sheets are 1220 by 610.

Clear acrylic in 0.5 & 1mm. Sheet sizes vary but some are 1000 by 1000mm.

MDF in: 1.5, 2, 3.2, 4 & 6mm, sheet sizes are 1220 by 600

The max size the cutter takes is about 350 by 450mm. The larger sheets will be cut down to approx. A3 or A4 and we will calculate the prices.

# Club Diary

July	4	Beaconsfield MRC exhibition, The Beaconsfield School, Beaconsfield
	17	Test Track
	18	Modelling Saturday
August	8	Modelling Saturday
	14	Test Track
September	1	<b>Start of R&amp;DMRC Financial Year Subscriptions Due</b>
	12	Modelling Saturday
	12-13	The International N gauge Show, Warwickshire Exhibition Centre, Fosse Way, Nr Leamington Spa, CV31 1XN
	18	Test Track
October	16	Test Track
	17	Club Annual Open Day, Community Centre, Princes Risborough
	17	Tad-Rail 2015, Cottesloe School, Wing
	23	Railway Talk by Geoff Plumb "That was the Year that was - 1962"
November	6	AGM (Annual General Meeting)
	7	Wycrail, Cressex Community School, High Wycombe
	13	Test Track
	14	Modelling Saturday
	28-29	Warley National Model Railway Exhibition, Hall 5, NEC, Birmingham
December	11	Test Track
	12	Modelling Saturday

Back page, photos from the Morris Collection,  
upper: **Hawker Siddeley prototype 'Kestrel' brand new ex-works. This photo was taken while 'Kestrel' was running round its train at Princes Risborough before heading back to Marylebone with a 'Big Nob Special' train laid on for the top Brass from BRB Headquarters at Marylebone (a nice little 'jolly' at taxpayers' expense it seems!)**

Lower: Metropolitan no 5 John Hampden at Rickmansworth.

